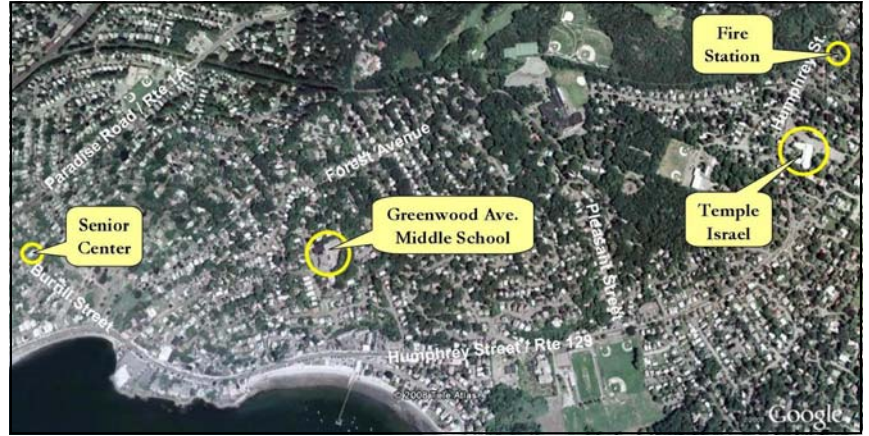


Swampscott TBOC DRAFT Design Guidelines

The Town Building Study Committee was formed by vote of the Spring Town Meeting. Its charge is to make recommendations for the disposition of four town owned properties identified as surplus by the Town Administration, Selectmen, and School Committee, as shown on the map below. This flyer includes draft Design Guidelines to govern the redevelopment of each property and will be included in Requests for Proposals (RFPs) to be issued for site disposition and redevelopment. The Design Guidelines will form the basis for decisions by developers on site and building design upon project completion. Proposals meeting the Design Guidelines will be more favorably viewed by the Town. Finally, the Town will enforce compliance with Design Guidelines during the permitting process.

The Committee will continue to refine this draft for completion prior to Town Meeting. However, this document provides a good background.



Fire Station on Phillips Avenue

The Fire Station on Phillips Avenue was built in 1903 with a distinctive and attractive design that incorporates stone, brick and shingles, resulting in a historic Craftsman character. The objective for the building is for it to be fully renovated and converted to single-family residential use. The renovations should incorporate the architectural features of the building, maintaining the key elements of the existing windows, doors, roof and cornice lines. The garage openings may be filled in with bays that are consistent with the overall character of the building. The new bays can be either windows or a main entrance to the building. A new garage of appropriate design may be added at the rear of the lot.



Recommended Use: single family residence.

Key points:

- Restoration of the historic Craftsman character required.
- No changes will be allowed to the existing building footprint or its overall structure, except to construct a two-car garage at the rear of the site and, if desired, to allow the rebuilding of the original tower (used to dry fire hoses).
- Existing garage bays may be filled in with bay windows and/or a new main entry designed to be consistent with the character of the existing building.
- Existing windows and doors to be repaired/restored to their original condition. Combination storm windows would be allowed.
- Replacing front parking area with landscaping may be appropriate.



Greenwood Avenue Middle School



The Greenwood School was built in 1895. It was substantially rebuilt and enlarged in 1936 with a Colonial Revival aesthetic. The site of the school is one of the highest elevation pieces of land between Swampscott and Boston, and offers excellent views of the ocean. The objective for the reuse of the property is to retain and renovate the original school building, demolish the 1936 additions, and to allow the construction of a new building at the rear of the site. The school is to be renovated in a historically sensitive way, with appropriate attention to existing windows, roof lines, doors, and pediments. Window openings should be fully utilized (without blank fill-ins). The new building would be no larger than and no higher than the Greenwood School, and should be designed to be compatible with the historic character of the existing school. The new building may include windows that are larger than those at the school so as to maximize the value of the views. Particular attention should be paid to the shape and form of the roof line of the new building.

Recommended Use: multi-family residential, maximum 41 units, including renovation of pre-1936 school building.

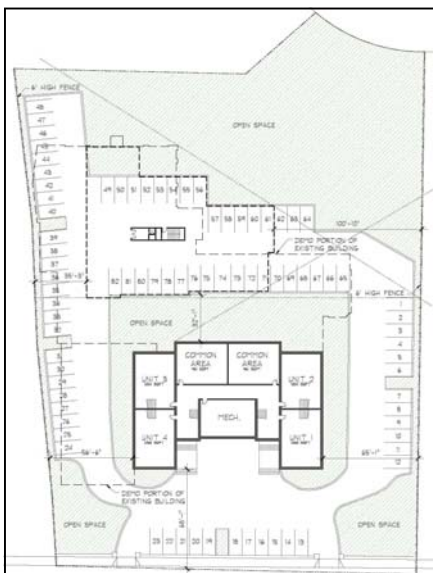


Key Points:

- The 1895 portion of the building would be required to be renovated.
- The newer portions of the existing building would be demolished, resulting in increased building setbacks from neighbors.
- Design and building materials of any new building would be required to be complementary to the historic design of the original school building. Large windows on new construction would be encouraged.
- Windows and doors of the 1895 building would be restored to the original openings without blank portions, and be visually appropriate to the historical structure and the brick detailing around the openings.
- Mechanical systems, including HVAC, would be hidden from view.
- The roof of any new building should be compatible with the hip style roof of the 1895 building, with no false façade roofs or flat roofs.
- Sidewalks are to be designed for pedestrian access and safety.
- The site and parking areas are to be screened from abutting properties.
- Existing trees and vegetation between Fuller Street and the building are to be retained and maintained with improvements to eliminate erosion, and to provide a sitting / picnic area and walking path to Fuller Street.

Illustrative site plan.

The schematic design drawing below was created by architects hired by the Town Building Study Committee. It represents a design option, and was completed as part of the Committee's site analysis. Bidders to redevelop the property will prepare their own design ideas, to be governed by Design Guidelines.



Temple Israel

The Temple Israel was purchased by the Town in June 2006. The buildings are currently vacant, and the Committee recommends redevelopment of the site with a maximum of 42 housing units in two buildings of not more than 3 stories in height. The surrounding neighborhood has many large single family homes and was developed with an interesting variety of architectural styles. New buildings designed for the site would differ in form, but should be compatible in design and detail work with the high quality architecture of these nearby homes, with articulations, materials, fenestration, and roof lines that provide a visual appearance compatible with the large homes on the surrounding streets. To the extent feasible, parking should be provided under the buildings.



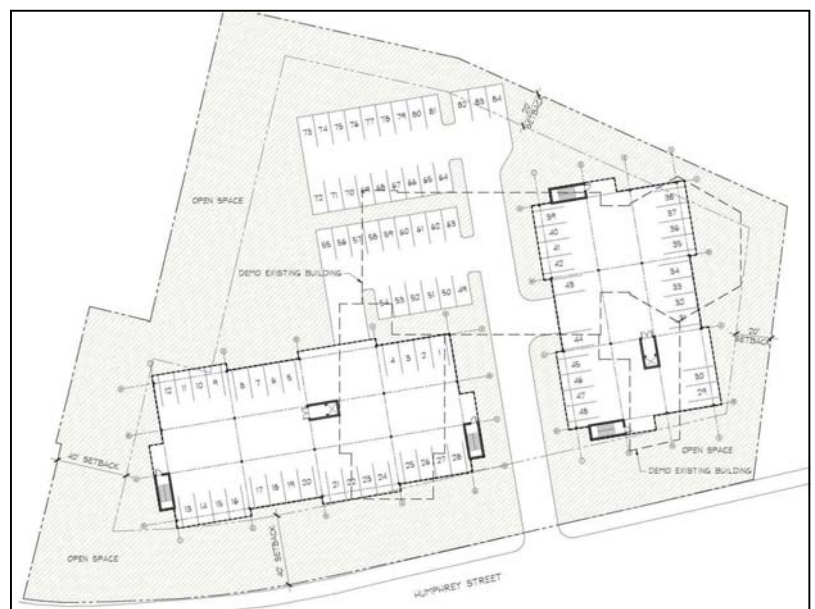
Recommended Use: multi-family, three stories, maximum 42 units.

Key Points:

- No portion of the existing building would be required to be preserved.
- Building design and detail elements should be compatible with and complementary to the existing residences in the area.
- The main entrances should face Humphrey Street.
- Garage entrances should not be visible from Humphrey Street.
- Roofs should not be flat or have false facades, except in areas where roof-top mechanical equipment is located, which would be screened.
- To the extent feasible, parking areas would be underground and/or under the building. On-grade parking would be behind the buildings relative to Humphrey Street.
- Sidewalks would be constructed to provide appropriate and safe pedestrian access and circulation.
- The site would be buffered from adjacent properties in a manner that is compatible with a primarily single-family neighborhood.
- The site should be landscaped, especially along the street frontage, in a manner that is compatible with a single-family neighborhood.

Illustrative site plan.

The schematic design drawing below was created by architects hired by the Town Building Study Committee. It represents a design option, and was completed as part of the Committee's site analysis. Bidders to redevelop the property will prepare their own design ideas, to be governed by Design Guidelines.



Senior Center

The Senior Center on Burrill Street was built in 1885. It contains 4,250 square feet of building space. Located near other public buildings, it was considered for potential demolition and construction of a parking lot. The Committee has rejected that option in favor of using the property for two or three units of affordable housing. The building's historical character should be maintained through renovations so that the structure continues to contribute to the charm of the neighborhood.

Key Points:

- Each of the units would be required to be affordable to families earning 80% or less of the area median income, with such restriction to last at least 30 years.
 - By banking industry lending standards and based on projected affordable buyer income, unit sale prices would be in the range of \$175,000 to \$210,000.
- The existing historical building is to be retained. No additions or character-changing alterations to the building's exterior would be allowed.
- Appropriate restoration of the historical details of the building is encouraged, in particular of the windows and doors, cornice, roof, chimney and front porch and deck details. Combination storm doors and windows are acceptable and encouraged for energy savings.
- The modern one-story addition at rear of building may be demolished.
- Exterior siding would be restored, repaired, or replaced with similar materials. No vinyl or aluminum siding would be allowed.
- A new window may be installed in the front façade for the attic.
- The existing pavement should be reduced to the minimum needed to accommodate off-street parking. The site should be landscaped appropriately, incorporating outdoor spaces for the residential units.



Recommended Use: multi-family residential, maximum 3 units.



The following principles would apply to all of the properties:

- Stormwater Management must incorporate Low Impact Development strategies such as drainage swales and bioretention basins to encourage stormwater infiltration, and minimize volume and improve quality of runoff.
- Existing public utility connections for each site would be maintained/improved.
- Zoning Amendments to be considered by Town Meeting will establish required parking ratios and dimensions.
- Outdoor lighting fixtures would be designed with a full cut-off and follow "dark skies" principles to ensure that no light would be shed across property lines.



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